

" THIS ARTICLE IS INTENDED FOR YOUR REFERENCE ONLY.

ACTUAL PARTS, YEARS AND BODY STYLES CONTAINED

IN THIS ARTICLE MAY DIFFER SLIGHTLY FROM YOUR APPLICATION. "

1955-57 INNER FIREWALL INSULATION PAD INSTALLATION



#06-97

This article will cover the installation of an original reproduction inner firewall insulation pad in a 1955 Chevy. The procedures for 1956-57 are similar, with the exception of the type of fasteners used to install the pad. When undertaking a full restoration, the firewall pad is the first thing to be installed after all the body and paint work is done. If your car is fully assembled and the original pad is in poor condition, it is possible to replace it after removing several important underdash components.

If you are building a modified or custom car, we recommend that you upgrade the insulation pad to the ABS type. It is far more durable, easier to clean and will really sharpen up your underdash area. The installation procedure for the ABS pads is similar to the original type.

Part Needed:

- 06-47 1955 Original Insulation Pad *
- 06-48 1956 Original Insulation Pad *
- 06-49 1957 Original Insulation Pad *
- 06-95 1955 ABS Insulation Pad *
- 06-96 1956 ABS Insulation Pad *
- 06-97 1957 ABS Insulation Pad *
- 34-109 1955 Insulation Pad Fastener
- 34-39 1956-57 Insulation Pad Fastener Set
- 49-09 1956-57 Insulation Pad Fastener Tool
- 09-07 1955-57 Firewall Floor Seal Automatic Trans.
- 09-10 1955-57 Firewall Floor Seal Standard Trans.
- 05-49 1955-56 Steering Column Rubber Floor Seal, Automatic Transmission
- 05-48 1955-56 Steering Column Rubber Floor Seal Standard Transmission
- 05-51 1957 Steering Column Rubber Floor Seal Automatic Transmission
- 05-50 1957 Steering Column Rubber Floor Seal Standard Transmission

** Additional shipping charges will apply due to heavy weight or oversize.*



#06-47



#09-10

#05-48



#49-09



#34-109 (each)



#34-39 (set)



#09-07

#05-49



Time Frame:

4 Hours

Tools Needed:

Insulation Pad Tool #49-09 (1956-57 Only)

#2 Phillips Screwdriver

Pliers

Box Knife

Ratchet & Misc. Sockets

**Photo #4 1955-56
Dash Panel Mat**

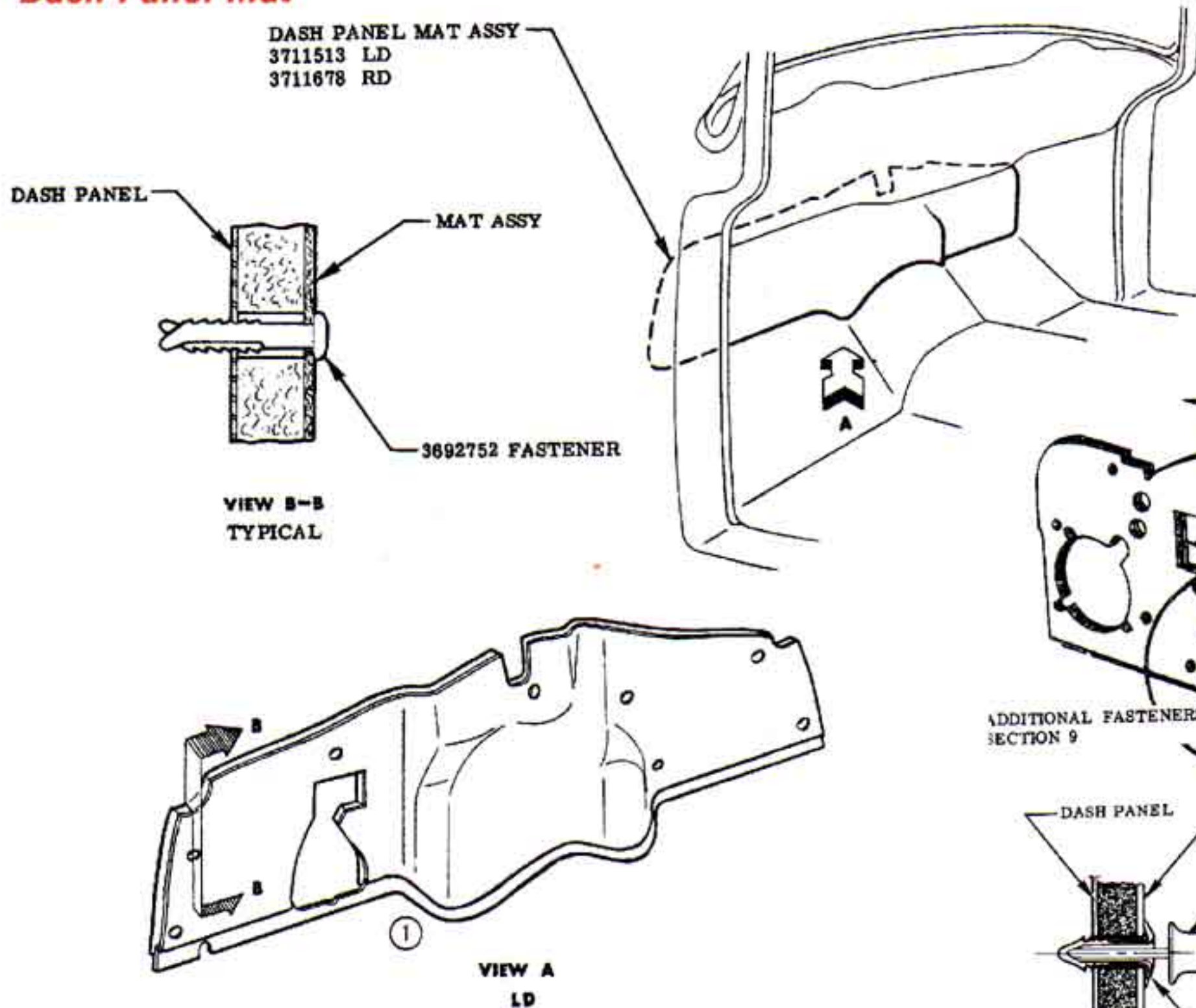


Photo #4: Remove the old insulation pad fasteners by using a pair of pliers or vise grips. There are a total of eight used on 1955-56 insulation pads with another five holding the rubber column seal. For 1957's, there are a total of nine used to attach the insulation pad with five used to secure the rubber column seal. After the old pad is removed, scrape out any old remaining insulation. If this area has substantial surface rust you may want to clean and prime while the pad and other underdash items are removed.

**Photo #4 1957
Dash Panel Mat**

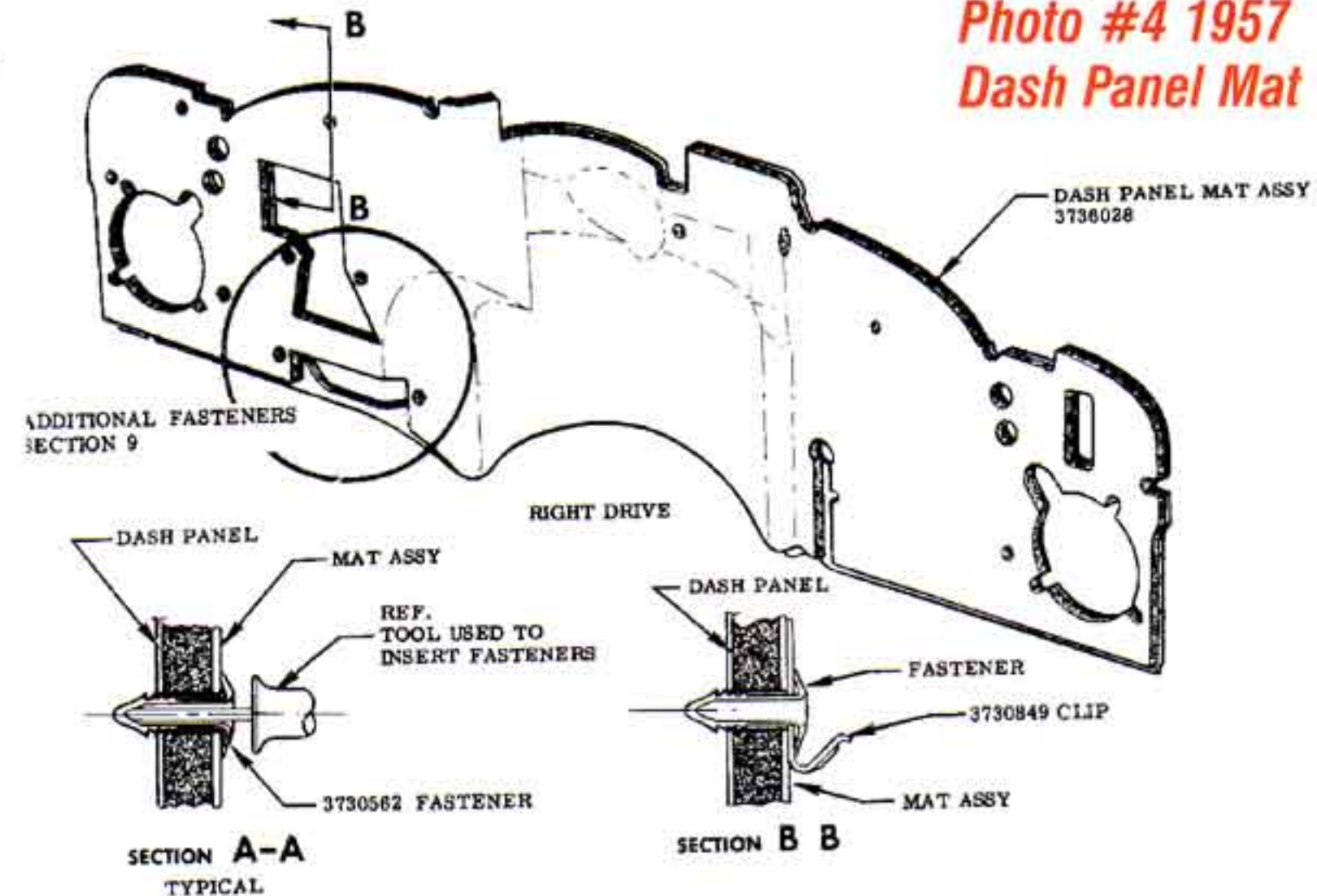
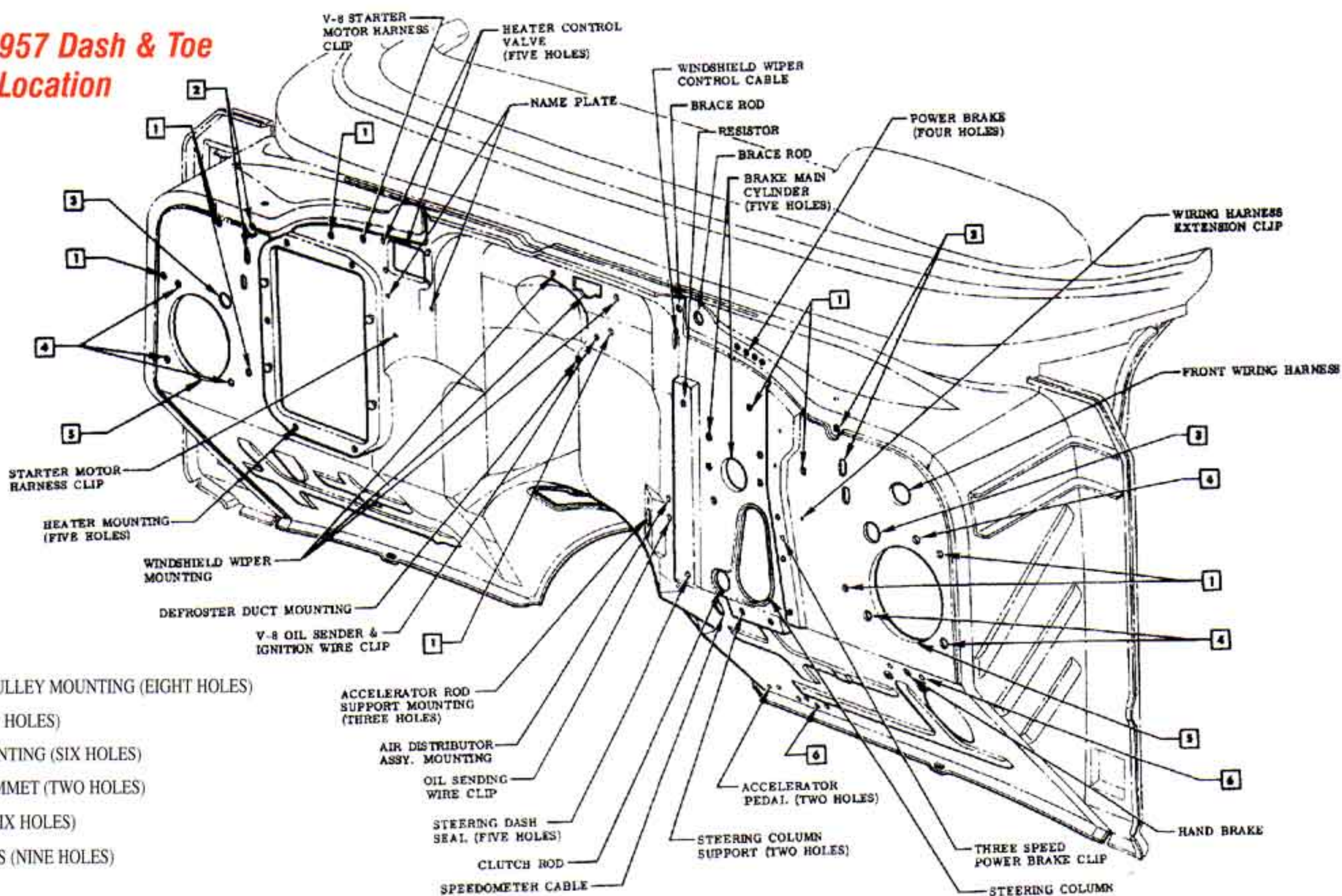


Photo #4 1957 Dash & Toe Panel Hole Location



- 6** HANDBRAKE PULLEY MOUNTING (EIGHT HOLES)
- 5** AIR VENT (TWO HOLES)
- 4** AIR DUCT MOUNTING (SIX HOLES)
- 3** AIR VENT GROMMET (TWO HOLES)
- 2** HOOD HINGE (SIX HOLES)
- 1** MAT FASTENERS (NINE HOLES)

ACCELERATOR ROD
SUPPORT MOUNTING
(THREE HOLES)

AIR DISTRIBUTOR
ASSY. MOUNTING

OIL SENDING
WIRE CLIP

STEERING DASH
SEAL (FIVE HOLES)

CLUTCH ROD

SPEEDOMETER CABLE

STEERING COLUMN
SUPPORT (TWO HOLES)

ACCELERATOR
PEDAL (TWO HOLES)

THREE SPEED
POWER BRAKE CLIP

STEERING COLUMN

HAND BRAKE



Photo #1: Begin by removing the front seat from the car. Also remove the glove box door, the glove box, any underdash wiring that passes through the firewall, vent and heater control cables.

Photo #2:

Remove the metal defroster duct from under the dash by removing the two sheet metal screws that attach it at each end.



Photo #3: On 1955-56 cars, it will be necessary to remove the master cylinder from the firewall and the underdash swing pedal bracket as well. On 1957 cars, the insulation pad is two-pieces so these items do not need to be removed. In order to complete the job properly, the steering wheel and column should be removed from the car. You may leave these in place, but the new floor seals that will be installed will have to be cut to allow fit around the base of the column.

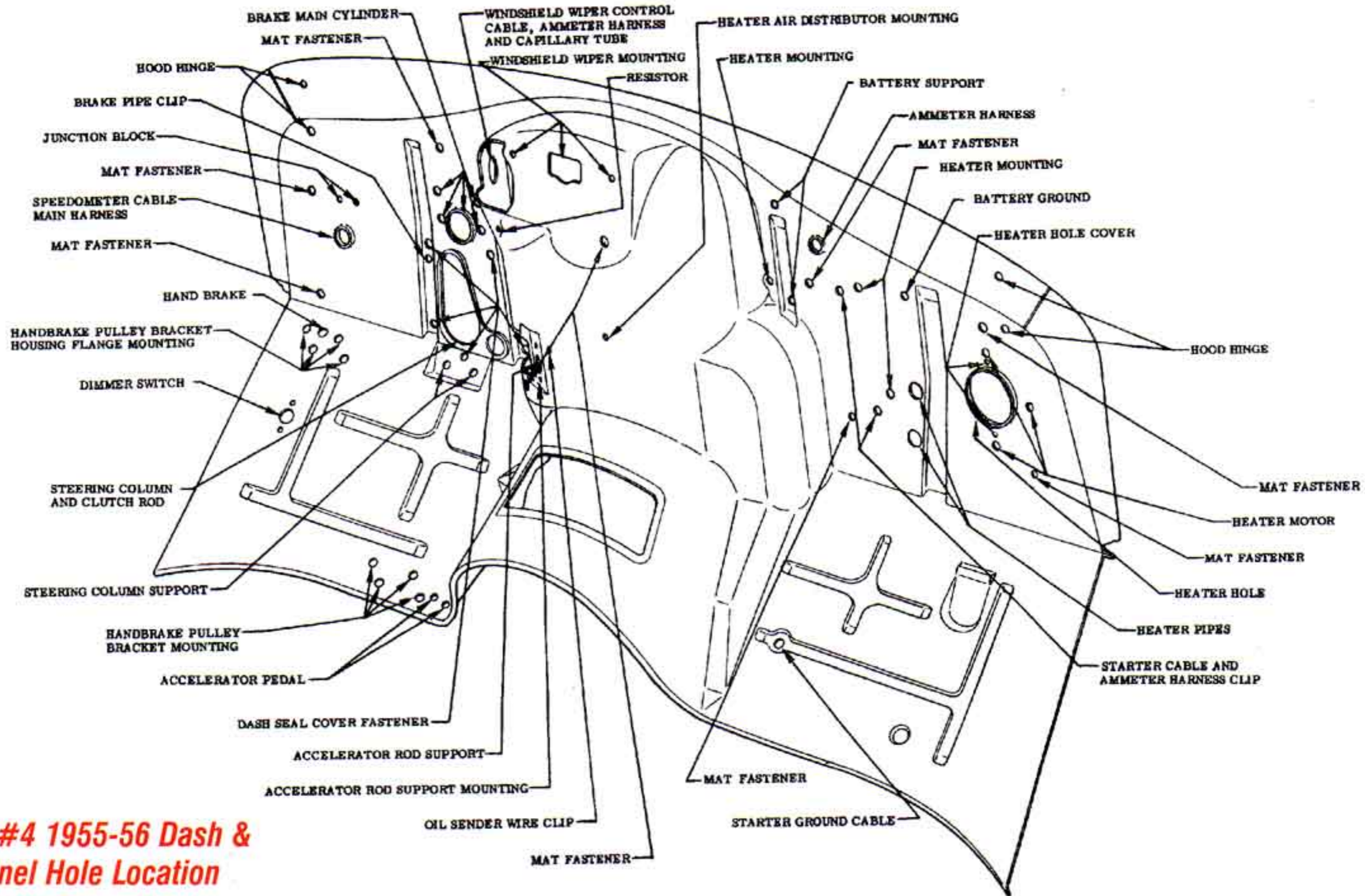


Photo #4 1955-56 Dash & Toe Panel Hole Location



Photo #5: Determine which of the “punch outs” in the new insulation pad will be used in your car. If you have deleted the fresh air vents or otherwise modified the firewall, you may wish to leave some of the pre-punched areas intact. The punch-outs needed are easily trimmed out with a box knife. Also be sure to punch out all of the fastener holes.

Photo #6: Position the new insulation pad and slide the accelerator hole in the pad over the rod in the firewall. If you are installing one of the original type pads, it is shipped flat and must be gently formed into shape as it fits the firewall. The ABS pads are pre-molded and do not require any additional shaping.



Photo #7: Using a #2 Phillips screwdriver or similar alignment tool, align each pad fastener hole with the related hole in the firewall. Holding the pad in place, insert the fasteners by hand one at a time to the first catch on the fastener. For 1955-57 cars, use tool #49-09 to push the plastic fasteners in. Do not force the fasteners all the way in just

yet. Repeat this for each hole in the pad until the pad fits properly and each of the fasteners is in place. Now that each

fastener is in place, each may now be pushed all the way in until flush. If the 1955 metal fasteners are stubborn, some gentle tapping with a hammer and a flat punch will make them go.

1957 Steering Column & Clutch Rod Dash Seal

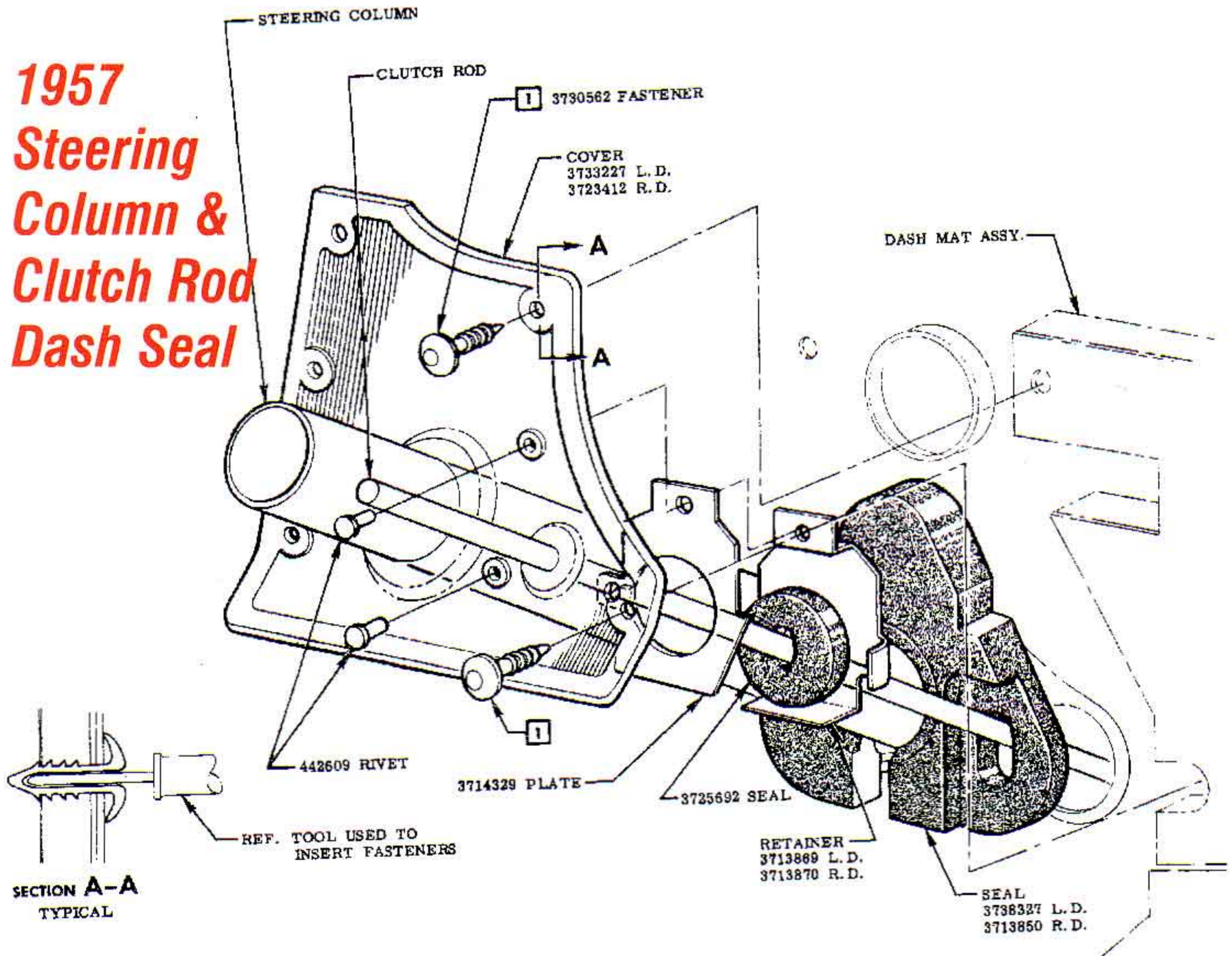




Photo #8: Next the appropriate rubber column seal and firewall floor seal should be slipped up over the bottom of the steering column. If the column is in the car, these two seals will need to be split with a sharp knife in order to be installed over the column. Secure these two seals to the firewall using the five remaining fasteners.



Photo #9: Once all the pad fasteners are in place, you may now install all of the items removed in the first few steps. The new pad will help reduce engine noise and heat inside the passenger area of your car, as well as increase the detail and value of your Classic. Good luck! ✓