

**" THIS ARTICLE IS INTENDED FOR YOUR REFERENCE ONLY.**

**ACTUAL PARTS, YEARS AND BODY STYLES CONTAINED**

**IN THIS ARTICLE MAY DIFFER SLIGHTLY FROM YOUR APPLICATION. "**

## 1957 REAR FIN AND CROWN MOLDING INSTALLATION



*The rear fin and crown moldings are the stainless steel moldings found on the top and rear of the quarter panels on 1957 150, 210 and Bel Air series cars. The crown moldings are the moldings on the top of the quarter panel, the crown molding on a 150 and 210 is short and the crown molding on a Bel Air reaches all the way to the B-pillar. The crown moldings on Bel Air Wagons and Nomads are slightly shorter than the regular passenger crowns. The crown moldings are not reproduced but the rear fin moldings and gas door have just become available as part #11-84. This will save a lot of hunting at swap meets and costly repair bills at the stainless polisher. In this article we will install the crown moldings, rear fin molding and gas door on a 210 wagon.*

### **Tools Needed:**

Phillips Screw Driver  
Hammer & Brass Punch  
WD-40

### **Time Frame:**

3 Hours

### **Parts Needed:**

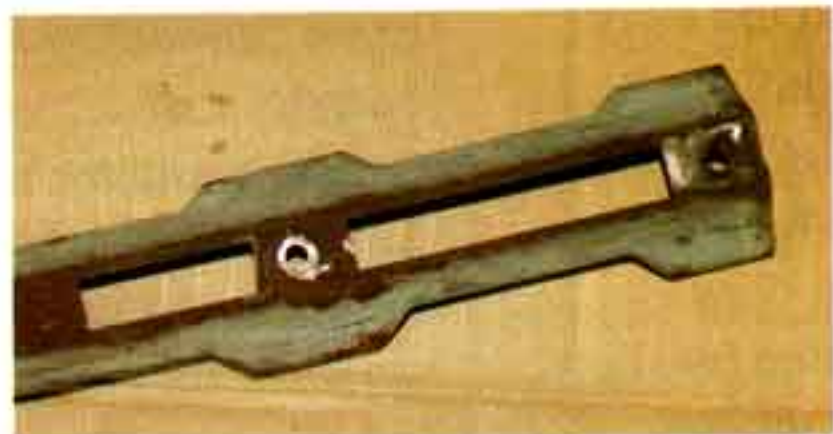
11-84	1957 Rear Fin And Gas Door Moldings
29-09	1957 Gas Door Bumper Set
34-161	1957 Rear Fin Molding Clips
34-106	1957 Crown Molding Stud (Long)
34-107	1957 Crown Molding Stud (Short)
34-44	1957 Crown Molding Clips (150/210 Series)
35-56	1957 Fuel Door Springs
34-116	1957 Fin Area Screw/Washer Set



**Photo #1:** The fin molding on a 150 or 210 series car is about 13" long. The moldings on a 150/210 or Bel Air are held to the top of the quarter panel in the same manner, a series of studs in the crown molding retainer and clips in the top of the quarter panel. The crown molding retainer is slid into the bottom of the molding from the rear.



**Photo #2:** The retainer will need to be removed to replace any broken studs. To remove the retainer use a hammer and a brass punch. Drive the retainer to the rear of the crown molding, a good blast of WD-40 really helps loosen up the retainer. Tap gently so you do not damage the stainless or the insert.



**Photo #3a & 3b:** The crown molding retainers for a 150/210 series car are not reproduced so the original retainer will be reused. The original studs were braded into the crown molding retainer. With a bench grinder or air file, cut the top off of the old studs and remove them from the retainer.



**Photo #4a & 4b:** There is a short and long crown molding stud, a 150/210 series car uses 1 short stud per side #34-107 and two long studs per side #34-106. A Bel Air uses two long studs per side and 9 short studs per side. On a 150/210 the short stud is used in the forward hole and the other holes use the long studs. The replacement studs are held in with a nut on the back side of the retainer.



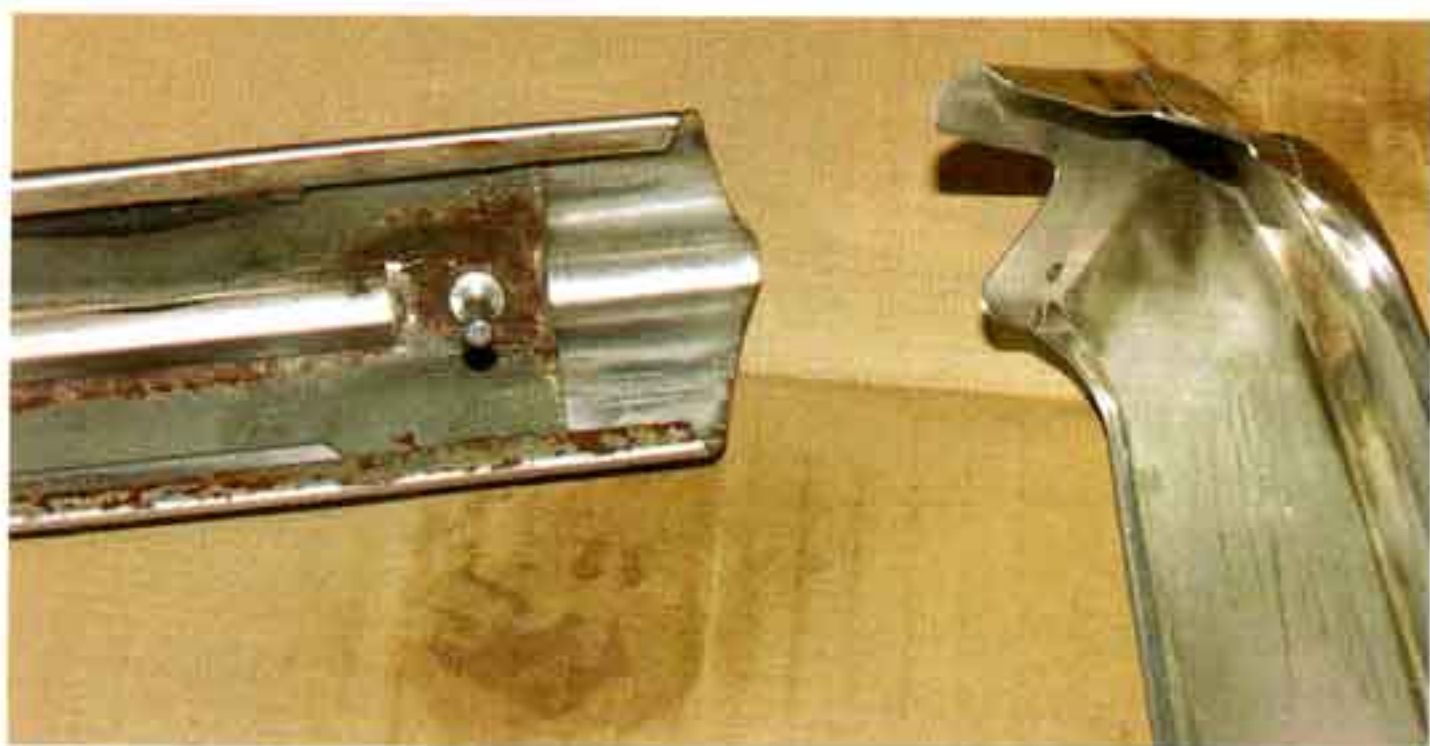
**Photo #5a & 5b:** To install the retainer back into the crown molding, place a small amount of grease on the edges of the retainer. This will help the retainer slide in easy. Using a hammer and brass punch, gently drive the retainer back into place. There will be marks in the crown moldings where the retainer was.



**Photo #6:** The crown molding clips #34-44 will snap into the square holes in the top of the quarter panel.



**Photo #7:** The fin molding is held to the rear of the quarter panel on each side with clips #34-161. This clip is held to the body with two sheet metal screws provided.



**Photo #8:** The crown molding and fin molding are coupled together with a tab that is spot welded to the top of the fin molding. These two pieces of stainless will just slide together as they are installed onto the body.



**Photo #9:** To install the two moldings on the car, first start with the fin molding, slide the fin molding half way down onto the clip on the rear of the quarter panel. Then couple the crown molding to the fin molding and work the two moldings together as you push down and forward. Some gentle bumping with the palm of your hand should force the moldings into place.



**Photo #10:** At the bottom of the fin molding there are two holes. Two #6 sheet metal screws included in set #34-116 are screwed into the rear of the quarter panel to hold the fin molding in place. If the holes in the quarter do not line up with the holes in the new fin molding, slight filing of the holes in the molding may be necessary.

**Photo #11:** #29-09 is the gas door bumper kit, this kit includes the two rubber sleeves that wrap around the two gas door hinges and the two bumpers and brackets that screw to the gas door. The two rubber sleeves will slide over the gas door hinges and keep the arms from chipping the paint. If the hinges on your gas door box are bent or broken, the entire assembly may be replaced with part #29-66 fuel door box with hinges.



**Photo #12a & 12b:** The gas door is held to the hinges with two clutch head machine screws and washers included in #34-116. The two gas door bumpers are held to the gas door with #6 phillips head sheet metal screws. These bumpers will prevent the edge of the gas door from chipping the paint on the outside of the quarter. Adjust the gas door until it fits the fin, the upper stainless and the taillight properly. If new gas door springs are needed use #35-56 spring set.





**Photo #13a & 13b:** The fin molding on the passenger side is one piece. Repeat the same process on the crown molding and install the fin and crown molding on the quarter panel. The fin molding is held to the back of the quarter with the upper fin clip and at the bottom with two #6 sheet metal screws from kit #34-116.



**Photo #14:** Check the fit of the new moldings and make any further adjustments needed for proper fit.

Good luck! 