

**" THIS ARTICLE IS INTENDED FOR YOUR REFERENCE ONLY.**

**ACTUAL PARTS, YEARS AND BODY STYLES CONTAINED**

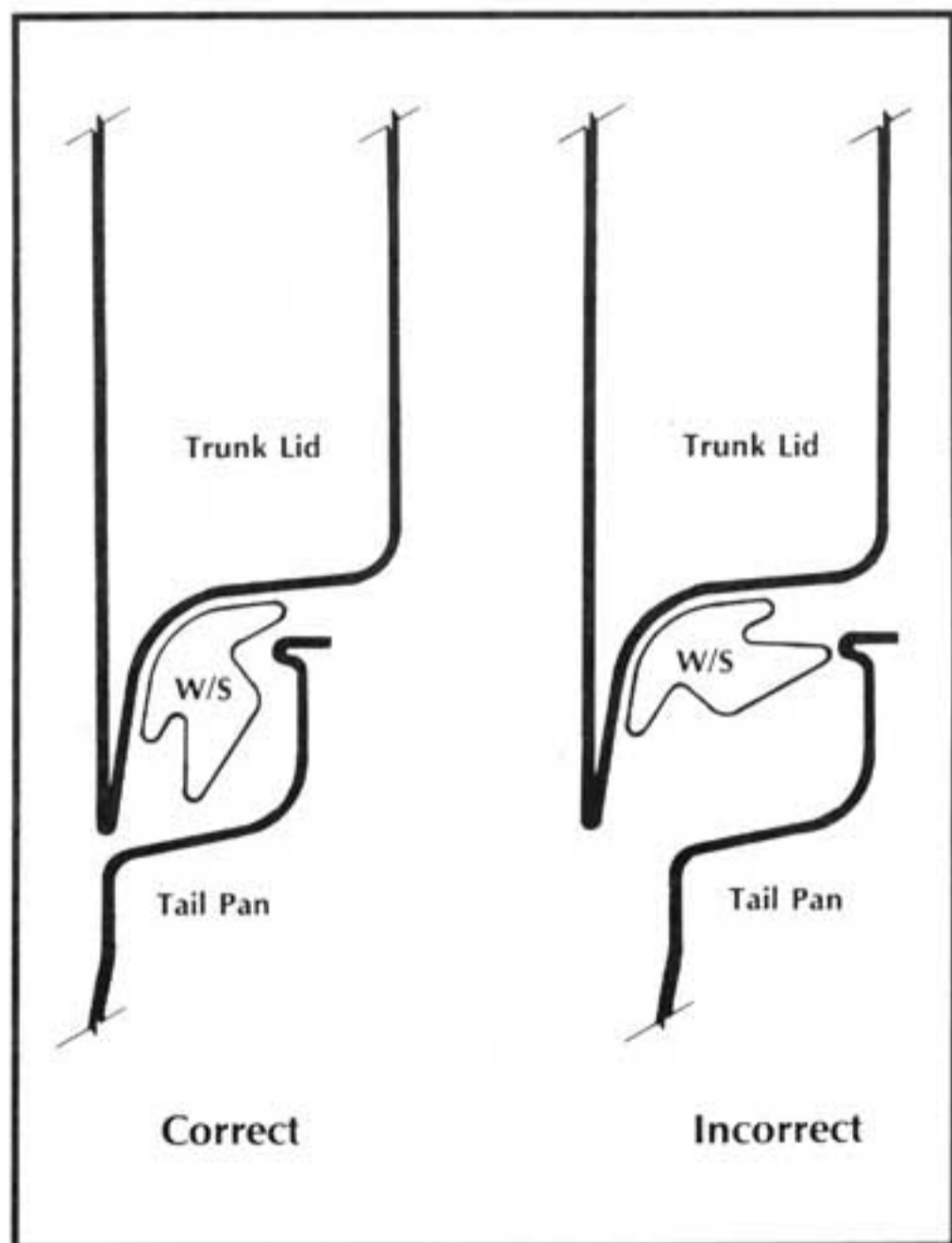
**IN THIS ARTICLE MAY DIFFER SLIGHTLY FROM YOUR APPLICATION. "**

# Tech Book

## TRUNK WEATHERSTRIP INSTALLATION

This Tech Article will explain the installation of the trunk weatherstripping for 1955-1957 passenger Chevys. It is extremely important to have a good trunk seal to help keep the tailpan and trunk area free of dust and excessive moisture which would cause the rusting away of this part of a Classic. (A misaligned trunk lid can also cause water and dust leaks.) Since the weatherstripping is made out of rubber material, it will deteriorate with the passing of time; and will become brittle and lose some of its shape. As a result from time to time, the replacement of this weatherstripping is required. The 1955 and 1956 trunk lids use a glue-type weatherstripping and the 1957 Chevy trunk lid uses a clip-type weatherstripping.

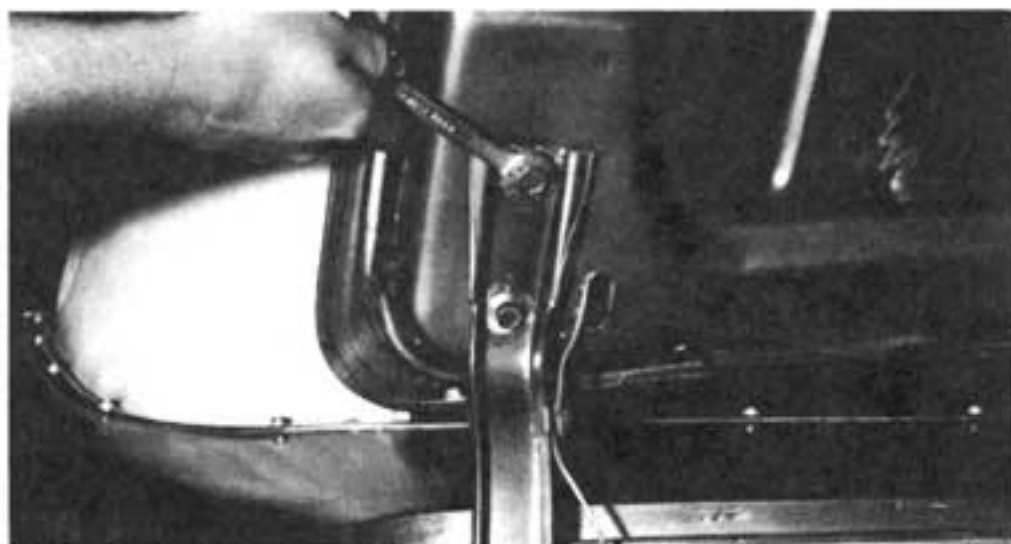
It is fairly easy to install this weatherstripping **incorrectly**, thus causing a bad seal and difficulty in closing the lid itself. Over the last year I have received a number of calls where Members had inadvertently installed the weatherstripping backwards and they were having difficulty closing the trunk lid. Below is a diagram showing the correct and the incorrect orientation of the weatherstripping.



The installation procedure used is very simple, very few tools are required, very few supplies are required, and it takes a relatively short time. I would not recommend removing the lid on the '57s because the installation of the weatherstripping can be accomplished quite easily with the trunk lid in place. The only exception to this would be if the lid had a trunk weatherstrip installed with glue and you needed to clean the trunk lid significantly to prepare it for the new installation. I have seen several very sloppy trunk weatherstrip installations on 1955's and 1956's. This is caused by attempting to glue the weatherstrip while the trunk lid is in place. Thus, I recommend that the trunk lids be removed on 1955's and 1956's before the installation of the trunk weatherstripping. The following procedure can be followed and a very nice, clean looking job will result. A big part of the success of this job depends on how clean the trunk lid is before the new weatherstrip is installed.

## Glue-type Weatherstrip Installation (1955/1956)

1. I highly suggest that the trunk lid is removed, not only for the cleaning of the old adhesive, but also for the ease of the installation of the new weatherstripping. After raising the trunk lid, place a protective covering over the paint finish of the trunk compartment edges.



2. Scribe the location of the hinge straps on the lid inner panel. This will be a big help when it comes time to reinstall the trunk lid. 3. Prepare a work area, preferably a table top, to place the trunk lid on. Place a piece of carpet, heavy cloth, or cardboard on the table to protect the trunk lid finish. 4. With the help of one or two friends, remove the four bolts which secure the hinge to the trunk lid. 5. Place the trunk lid on the protected table top so that the underside of the lid is facing up.



6. Using a putty knife, remove all of the old weatherstripping. 7. A very good solvent to use throughout this job is 3M's "General Purpose Adhesive Cleaner," Part #08984, which can be found at an auto paint supply store. A quart can of this cleaner is available through the Club as Part #49-5.

Using the solvent and a putty knife, clean any remaining adhesive. The old adhesive does not come off very easily, thus a small wire brush can be used for the difficult spots. 8. Depending on how you clean the area where the weatherstrip will be installed, will determine how you prepare the metal for the installation. Weatherstrip adhesive and the weatherstrip itself should not be installed on bare metal, thus this area of the trunk lid needs to be primed and painted. 9. Now the lid is ready for the installation of the weatherstrip. The weatherstrip will fit two ways in the trunk lid notch, one is the correct orientation and the other orientation will not work. Consult the diagram on this page for the proper location and orientation of the weatherstrip. This should leave the longer, sealing lip pointing toward the body, pointing away from the trunk lid. 10. The two ends of the weatherstrip should meet at the "bottom" of the trunk lid. Originally this union was located about 12 to 16 inches to the passenger side of the trunk lock. If you choose a different location for the ends, I suggest not selecting any area along the "top" of the lid as this may result in a leak. 11. An excellent adhesive to use for this job is 3M's "Super Weatherstrip Adhesive," Part #08001, which can be found at auto paint supply stores. A 5 oz. tube is available through the Club as Part #49-06.

Since this is a fairly fast drying adhesive, you will not want to apply the adhesive to the entire length of the weatherstrip at one time.



**12.** The correct method of using the adhesive is to apply a coat to the weatherstripping and a coat to the trunk lid, then to press the weatherstrip into place. I suggest spreading a coat of adhesive to the middle 1/2-inch of the two contacting surfaces. This will keep the job very clean, because excessive adhesive would ooze out of the sides and leave a mess which is difficult to clean up. You don't need a continuous "bead" of adhesive, intermittent 2-inch "beads" will do the job. **13.** Install the weatherstripping about 12 inches at a time. Clean any mess you make before the adhesive dries. **14.** Continue to install the weatherstrip until there is approximately six inches of trunk lid left. (You should have several inches of extra weatherstripping.)



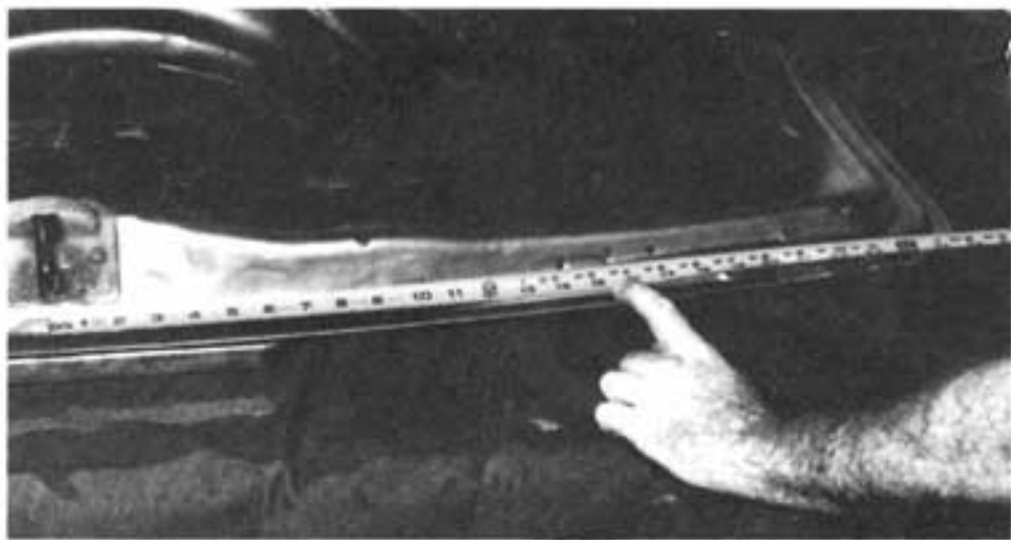
**15.** Carefully measure and mark the exact location for the end of the weatherstrip. Do not stretch the weatherstrip when determining where to cut because the weatherstrip will "spring" back, leaving it too short. If anything, cut the weatherstrip 1/16-inches too long. Use something very sharp, such as an X-acto knife or a razor blade to cut the weatherstrip. Make the cut so that the ends will join without leaving any gap between them.



**16.** Gluing the ends together is where this job can become very messy, and yet there needs to be sufficient adhesive used to form a permanent union. Carefully place some adhesive onto both ends of the weatherstrip, smearing the adhesive around so that both ends are completely covered. Do not allow the ends to touch each other yet. **17.** In the manner previously described, place adhesive on the trunk lid and weatherstripping so that the end of the weatherstrip can be glued in place. **18.** Squeeze the two ends of the weatherstrip together and hold the union for about one minute. **19.** Using some adhesive cleaner and a rag, clean any excessive adhesive left on the trunk lid and weatherstripping. Be careful not to get too much of the cleaner on the adhesive at the union of the two ends. **20.** A thin coating of silicone lubricant can be sprayed onto the weatherstrip to help keep the rubber supple and to keep it from sticking to the trunk body when closed. Protect the paint finish from the spray by using paper and tape, or a piece of cardboard. **21.** Using the four retaining bolts and some assistance, reinstall the trunk lid. Use the scribe marks to relocate the lid properly.

### Clip-type Weatherstrip Installation (1957)

22. Using a screwdriver, pry out the clips securing the old weatherstripping. 23. Prepare the trunk lid for the new weatherstrip in the manner described in steps 7 and 8.



24. After consulting the diagram on page 7 for the correct orientation of the weatherstrip, start to install the weatherstrip so that the first clip to be installed is in one of the two holes with the odd spacing. The normal spacing is about 4 inches, the odd spacing was 2 1/2 inches. 25. Press the first clip into position using your thumb, pressing directly over the nylon clip. The nylon clips have sharp edges and can easily cut through the weatherstripping if these aren't pressed directly inward.



26. Continue to install the weatherstrip around the trunk lid. 27. If there is one spot where the weatherstrip puckers up due to an excessive gap between clips, a small amount of adhesive can be used to pull the weatherstrip into place.



28. The 1957 trunk weatherstrip has a circular tunnel which runs the length of the weatherstrip. I glued a 1 1/2-inch long piece of wiper vacuum hose into the two joining ends of the weatherstrip. This tubing acts like a backbone in tying the two loose ends of the weatherstrip together. 29. Cut and glue the two ends of the weatherstrip together using the procedure described in steps 15, 16, 18, 19, and 20. This installation is so easy, it is too bad that the 1955/1956 weatherstrip is not the clip-type.

